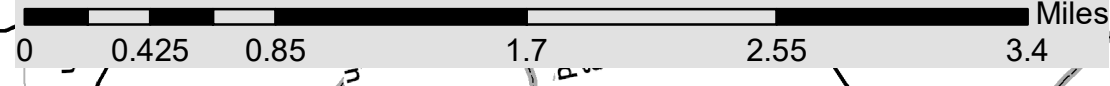
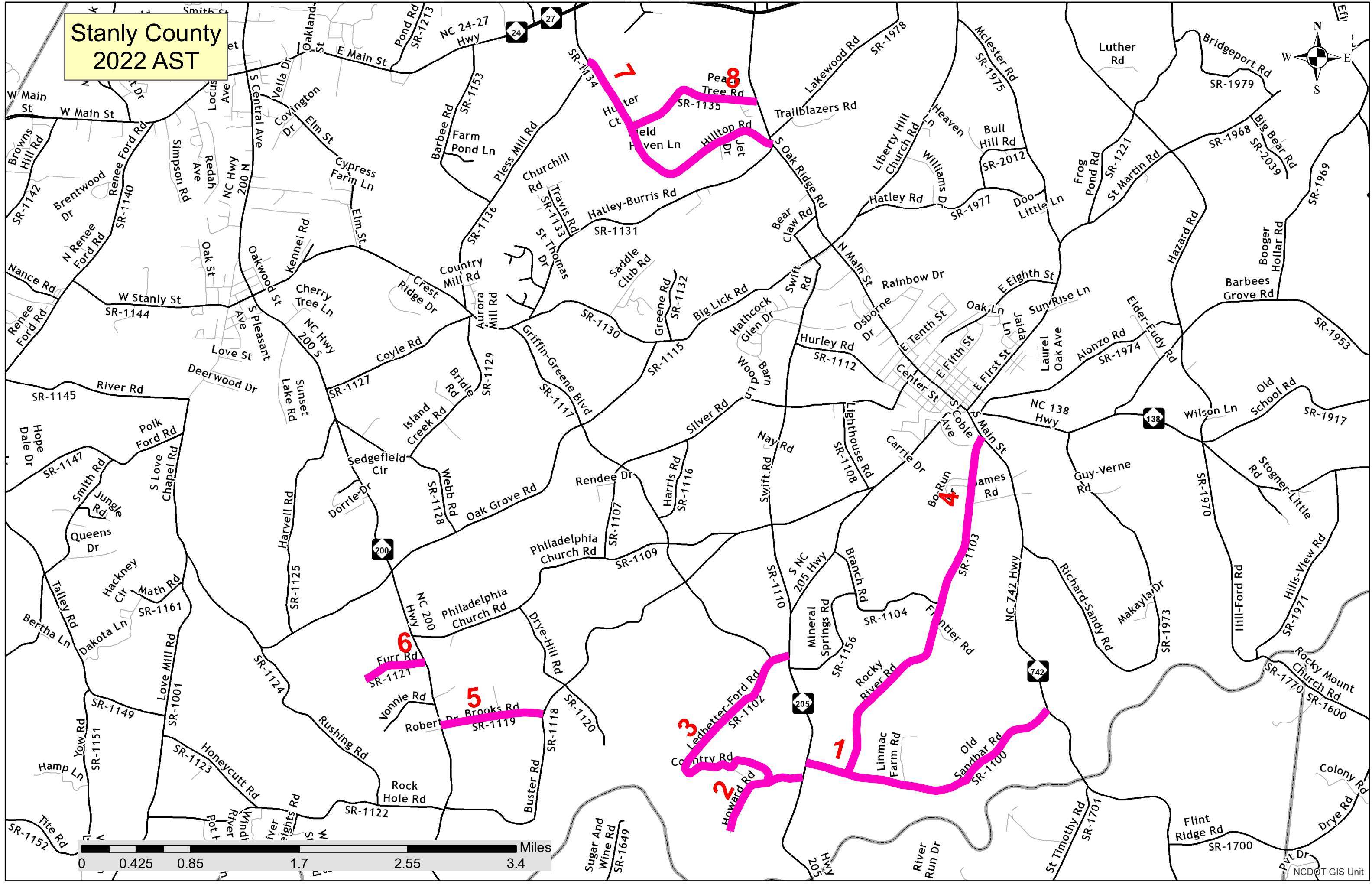
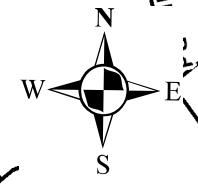
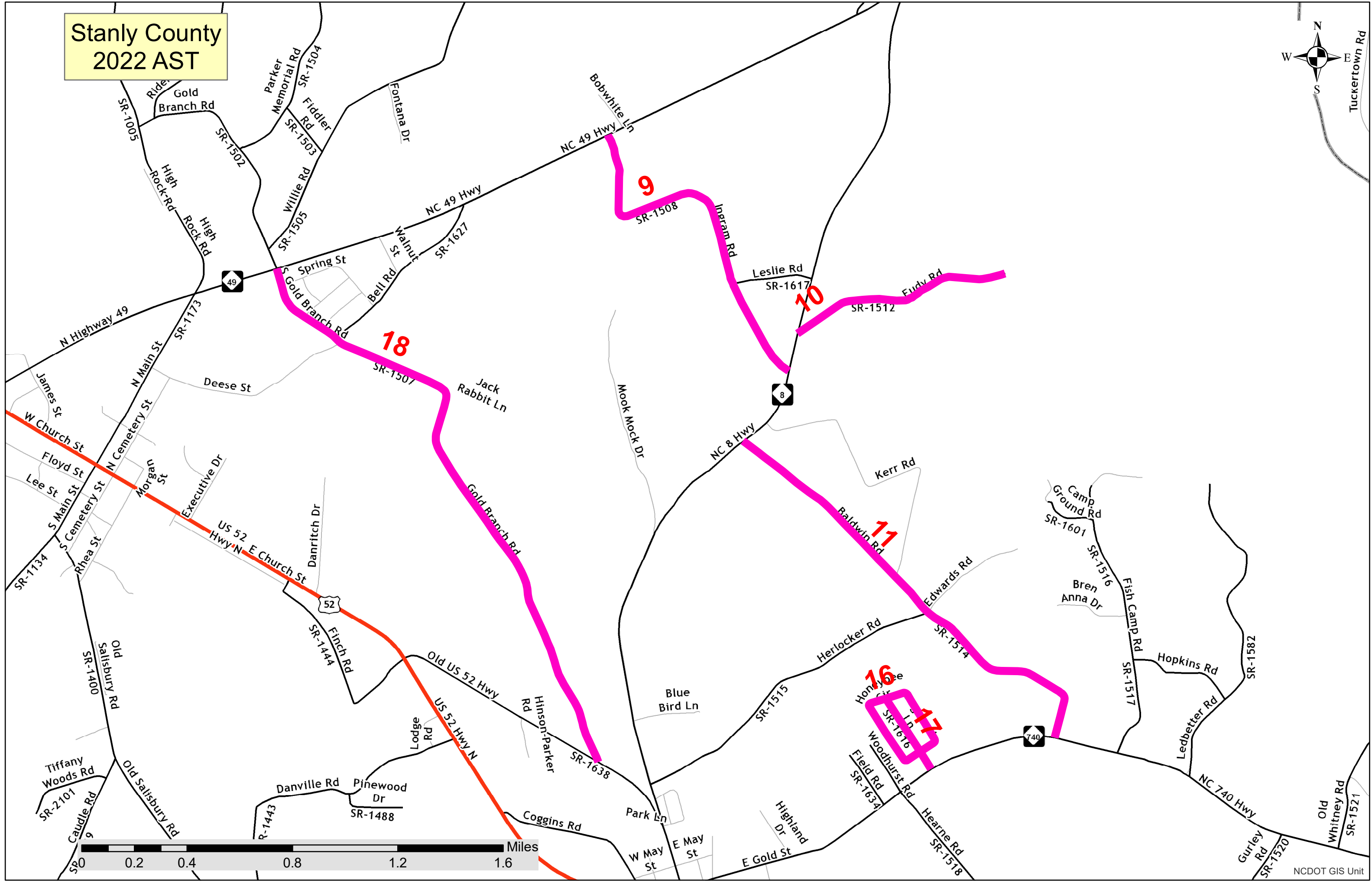
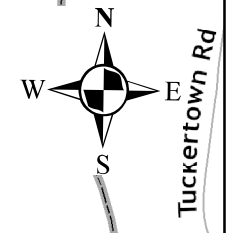


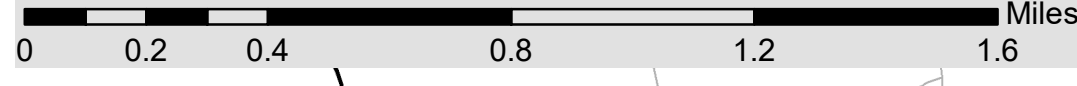
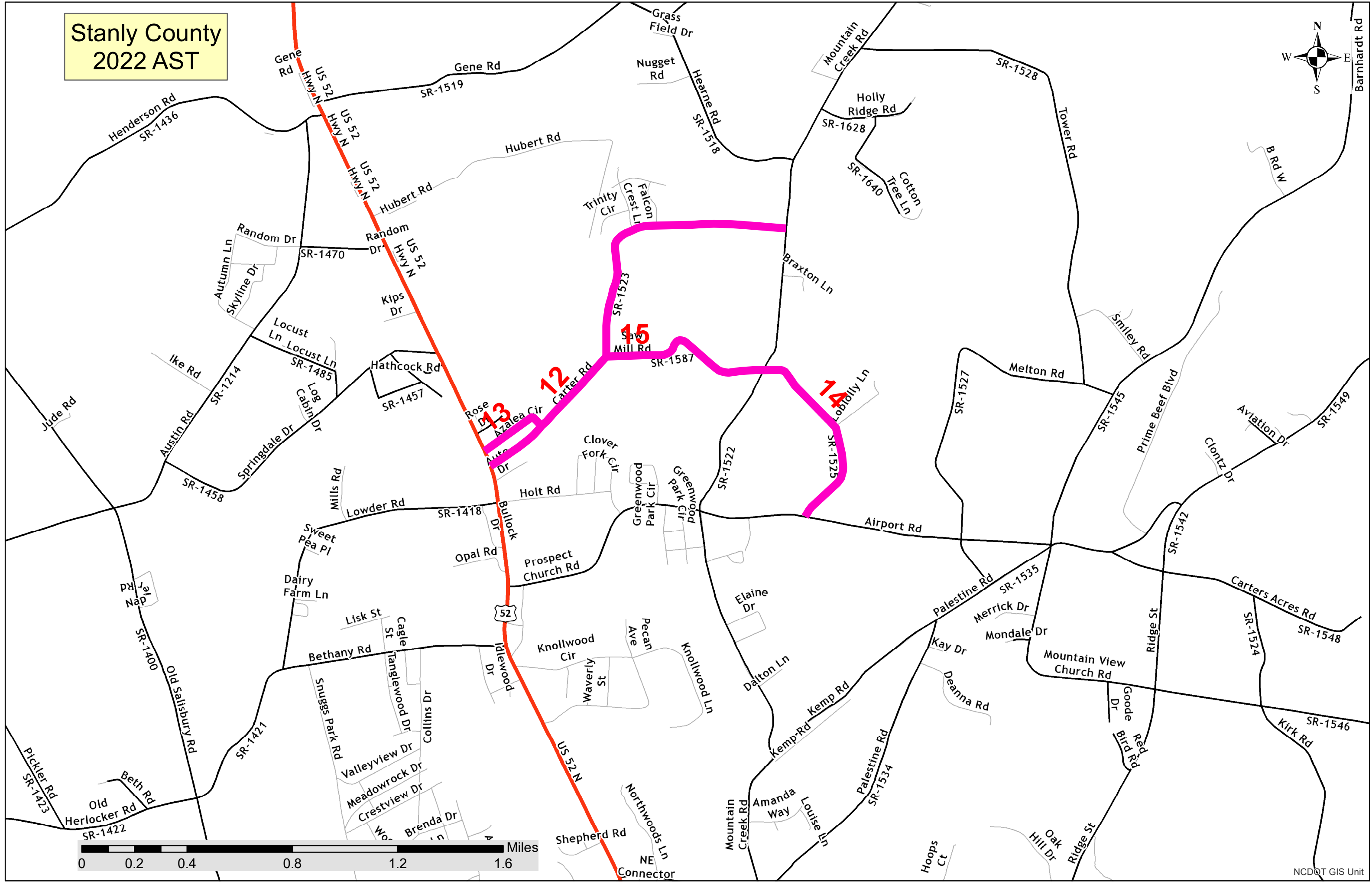
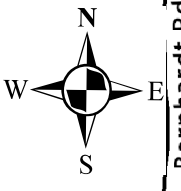
# Stanly County 2022 AST



# Stanly County 2022 AST

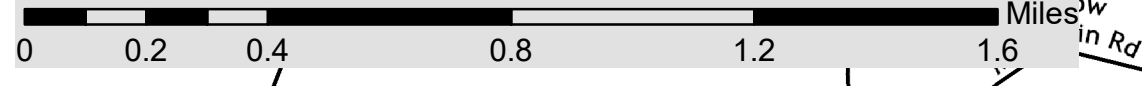
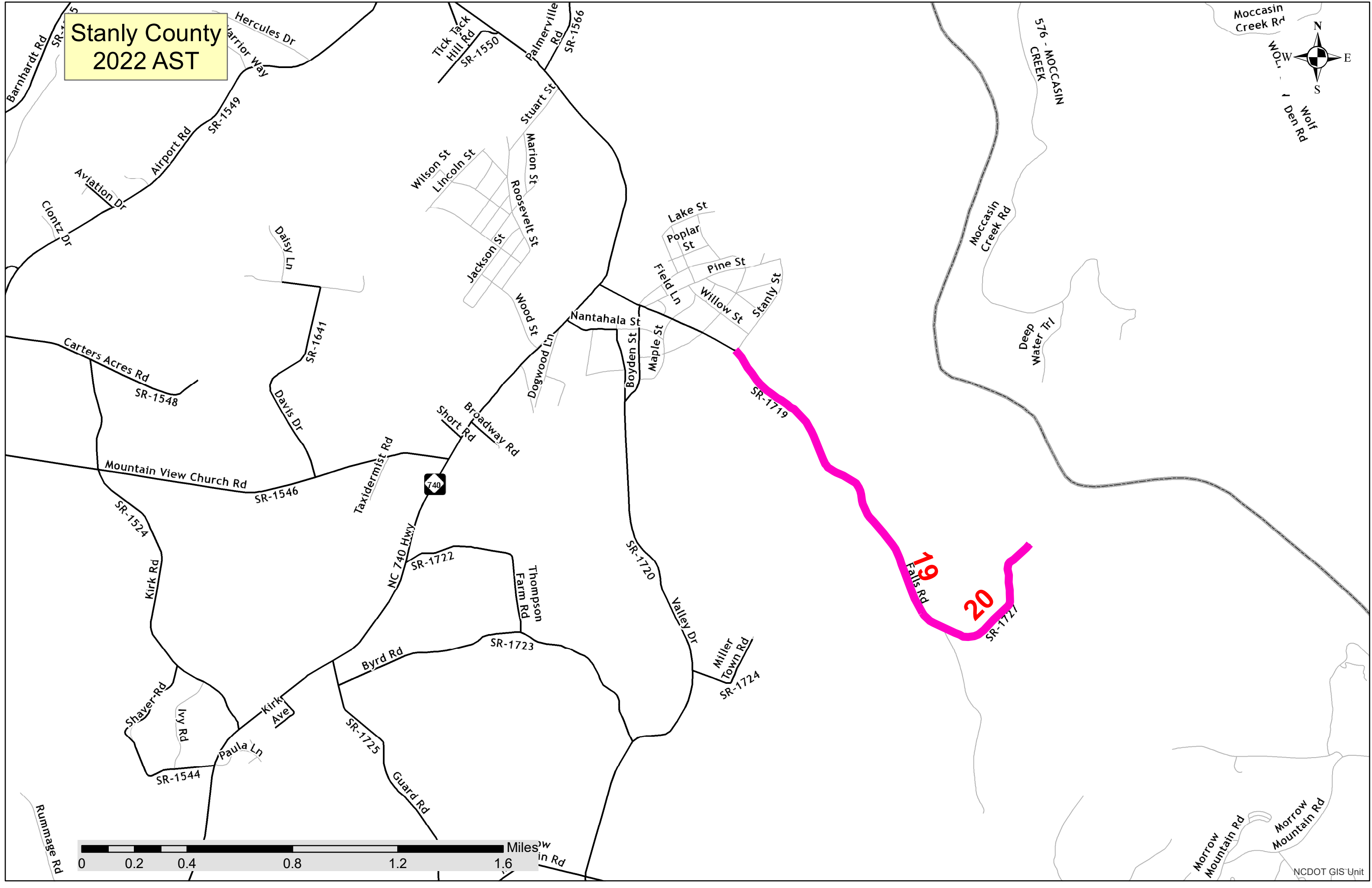
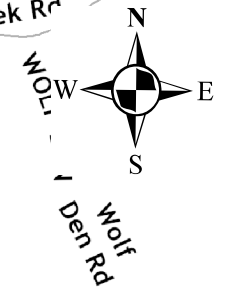


**Stanly County  
2022 AST**





**Stanly County  
2022 AST**



PROJECT NO.	SHEET NO.	TOTAL NO.
2022CPT.10.04.20842	1	8

## SUMMARY OF QUANTITIES

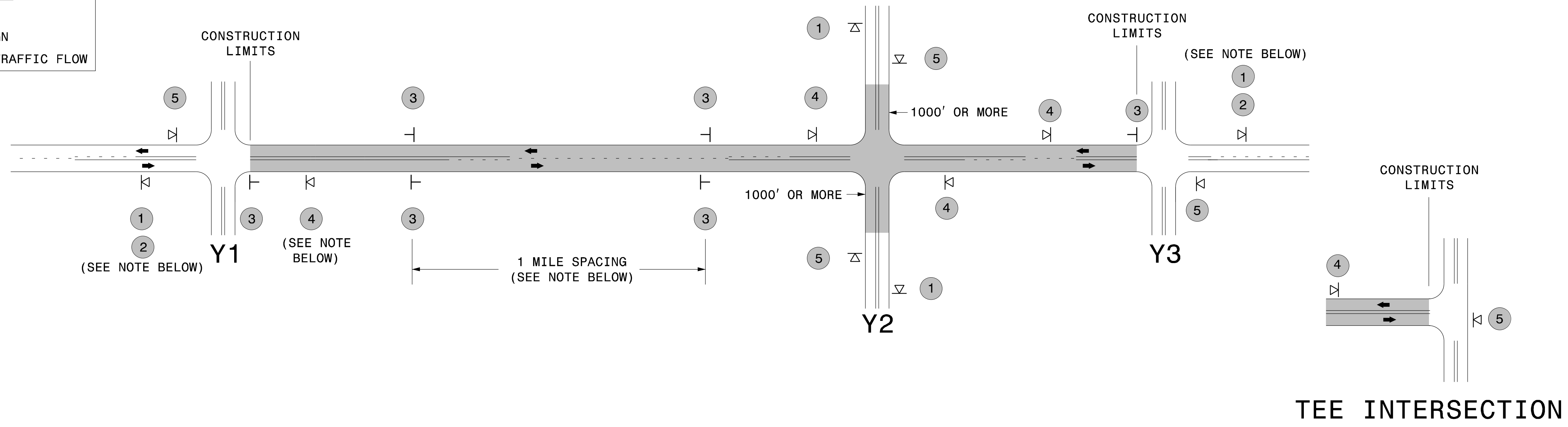
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	FROM MP	TO MP	LANES	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	1803500000-E	1838000000-E	1838500000-N	2143000000-E	4413000000-E	4457000000-N		
												ASPHALT SURFACE TREATMENT, DOUBLE SEAL	EMULSION FOR ASPHALT SURFACE TREATMENT	VACUUM TRUCK	BLOTTING SAND	WORK ZONE ADVANCE/GENERAL WARNING SIGNING	TEMPORARY TRAFFIC CONTROL		
												MI	FT	SY	GAL	WK	TON	SF	LS
2021CPT.10.16.20842	Stanly	1	OLD SANDBAR RD (SR 1100)	FROM NC 742 TO NC 205	0.00	2.17	2	NO	NO	2.17	18	23,374	12,855			110			
<b>TOTAL FOR MAP NO. 1</b>												<b>2.17</b>		<b>23,374</b>	<b>12,855</b>			<b>110</b>	
2021CPT.10.16.20842	Stanly	2	HOWARD RD (SR 1101)	FROM NC 205 TO DEAD END	0.00	0.82	2	NO	NO	0.82	20	9,814	5,398			40			
<b>TOTAL FOR MAP NO. 2</b>												<b>0.82</b>		<b>9,814</b>	<b>5,398</b>			<b>40</b>	
2021CPT.10.16.20842	Stanly	3	COUNTRY RD/LEDBETTER FORD RD (SR 1102)	FROM HOWARD RD (SR 1101) TO NC 205	0.00	1.88	2	NO	NO	1.88	20	22,500	12,375			40			
<b>TOTAL FOR MAP NO. 3</b>												<b>1.88</b>		<b>22,500</b>	<b>12,375</b>			<b>40</b>	
2021CPT.10.16.20842	Stanly	4	ROCKY RIVER RD (SR 1103)	FROM NC 742 TO OLD SANDBAR RD (SR 1100)	0.00	2.98	2	NO	NO	2.98	20	35,665	19,616			182			
<b>TOTAL FOR MAP NO. 4</b>												<b>2.98</b>		<b>35,665</b>	<b>19,616</b>			<b>182</b>	
2021CPT.10.16.20842	Stanly	5	BROOKS RD (SR 1119)	FROM NC 200 TO BUSTER RD (SR 1118)	0.00	0.81	2	NO	NO	0.81	20	9,694	5,332			40			
<b>TOTAL FOR MAP NO. 5</b>												<b>0.81</b>		<b>9,694</b>	<b>5,332</b>			<b>40</b>	
2021CPT.10.16.20842	Stanly	6	FURR RD (SR 1121)	FROM NC 200 TO DEAD END	0.00	0.50	2	NO	NO	0.5	20	5,984	3,292			40			
<b>TOTAL FOR MAP NO. 6</b>												<b>0.5</b>		<b>5,984</b>	<b>3,292</b>			<b>40</b>	
2021CPT.10.16.20842	Stanly	7	HILLTOP RD (SR 1134)	FROM NC 205 TO PLESS MILL RD (SR 1136)	0.00	2.05	2	NO	NO	2.05	20	24,534	13,494			110			
<b>TOTAL FOR MAP NO. 7</b>												<b>2.05</b>		<b>24,534</b>	<b>13,494</b>			<b>110</b>	
2021CPT.10.16.20842	Stanly	8	PEACH TREE RD (SR 1135)	FROM NC 205 TO HILLTOP RD (SR 1134)	0.00	1.11	2	NO	NO	1.11	20	13,284	7,306			40			
<b>TOTAL FOR MAP NO. 8</b>												<b>1.11</b>		<b>13,284</b>	<b>7,306</b>			<b>40</b>	
2021CPT.10.16.20842	Stanly	9	INGRAM RD (SR 1508)	FROM NC 49 TO NC 8	0.00	1.40	2	NO	NO	1.4	20	16,755	9,035			40			
<b>TOTAL FOR MAP NO. 9</b>												<b>1.4</b>		<b>16,755</b>	<b>9,035</b>			<b>40</b>	
2021CPT.10.16.20842	Stanly	10	EUDY RD (SR 1512)	FROM NC 8 TO END OF PAVEMENT	0.00	0.85	2	NO	NO	0.85	20	10,173	5,595			40			
<b>TOTAL FOR MAP NO. 10</b>												<b>0.85</b>		<b>10,173</b>	<b>5,595</b>			<b>40</b>	
2021CPT.10.16.20842	Stanly	11	BALDWIN RD (SR 1514)	FROM NC 8 TO NC 740	0.00	1.75	2	NO	NO	1.75	18	18,850	10,367			40			
<b>TOTAL FOR MAP NO. 11</b>												<b>1.75</b>		<b>18,850</b>	<b>10,367</b>			<b>40</b>	
2021CPT.10.16.20842	Stanly	12	CARTER RD (SR 1523)	FROM US 52 TO MT CREEK RD (SR 1522)	0.00	1.71	2	NO	NO	1.71	18	18,419	10,131			40			
<b>TOTAL FOR MAP NO. 12</b>												<b>1.71</b>		<b>18,419</b>	<b>10,131</b>			<b>40</b>	
2021CPT.10.16.20842	Stanly	13	AZALEA CIR (SR 1577)	FROM CARTER RD (SR 1523) TO US 52	0.00	0.27	2	NO	NO	0.27	18	2,908	1,599			40			
<b>TOTAL FOR MAP NO. 13</b>												<b>0.27</b>		<b>2,908</b>	<b>1,599</b>			<b>40</b>	
2021CPT.10.16.20842	Stanly	14	SAW MILL RD (SR 1525)	FROM AIRPORT RD (SR 1524) TO MT CREEK RD (SR 1522)	0.00	0.76	2	NO	NO	0.76	20	9,096	5,003			40			
<b>TOTAL FOR MAP NO. 14</b>												<b>0.76</b>		<b>9,096</b>	<b>5,003</b>			<b>40</b>	
2021CPT.10.16.20842	Stanly	15	SAW MILL RD (SR 1587)	FROM MT CREEK RD (SR 1522) TO CARTER RD (SR 1523)	0.00	0.66	2	NO	NO	0.66	20	7,899	4,344			40			
<b>TOTAL FOR MAP NO. 15</b>												<b>0.66</b>		<b>7,899</b>	<b>4,344</b>			<b>40</b>	
2021CPT.10.16.20842	Stanly	16	HONEY BEE CIR (SR 1615)	FROM DOGWOOD LN (SR 1616) TO DOGWOOD LN (SR 1616)	0.00	0.75	2	NO	NO	0.75	18	8,078	4,443			40			
<b>TOTAL FOR MAP NO. 16</b>												<b>0.75</b>		<b>8,078</b>	<b>4,443</b>			<b>40</b>	
2021CPT.10.16.20842	Stanly	17	DOGWOOD LN (SR 1616)	FROM NC 740 TO HONEY BEE CIR (SR 1615)	0.00	0.32	2	NO	NO	0.32	18	3,447	1,896			40			
<b>TOTAL FOR MAP NO. 17</b>												<b>0.32</b>		<b>3,447</b>	<b>1,896</b>			<b>40</b>	
2021CPT.10.16.20842	Stanly	18	GOLD BRANCH RD (SR 1507)	FROM NC 49 TO N MAIN ST (SR 1638)	0.00	2.40	2	NO	NO	2.4	20	28,723	15,798			158			
<b>TOTAL FOR MAP NO. 18</b>												<b>2.4</b>		<b>28,723</b>	<b>15,798</b>			<b>158</b>	
2021CPT.10.16.20842	Stanly	19	FALLS RD (SR 1719)	FROM END OF CURB TO END OF MAINTENANCE	0.58	1.84	2	NO	NO	1.26	20	15,080	8,294			40			
<b>TOTAL FOR MAP NO. 19</b>												<b>1.26</b>		<b>15,080</b>	<b>8,294</b>			<b>40</b>	
2021CPT.10.16.20842	Stanly	20	FALLS RD (SR 1719)	FROM FALLS RD (SR 1719) TO DEAD END	0.00	0.67	2	NO	NO	0.67	20	8,019	4,410			40			
<b>TOTAL FOR MAP NO. 20</b>												<b>0.67</b>		<b>8,019</b>	<b>4,410</b>			<b>40</b>	
<b>TOTAL FOR PROJ NO. 2021CPT.10.16.20842</b>												<b>25.12</b>		<b>292,296</b>	<b>160,583</b>	<b>3</b>	<b>10.00</b>	<b>1,200</b>	<b>1</b>
<b>GRAND TOTAL</b>												<b>25.12</b>		<b>292,296</b>	<b>160,583</b>	<b>3</b>	<b>10.00</b>	<b>1,200</b>	<b>1</b>

NOTE: The Contractor shall not park equipment or work outside of the Right Of Way in Historic Areas.

# SIGNING FOR ASPHALT SURFACE TREATMENT

**LEGEND**

- ◻ PORTABLE SIGN
- ┌ STATIONARY SIGN
- ← DIRECTION OF TRAFFIC FLOW



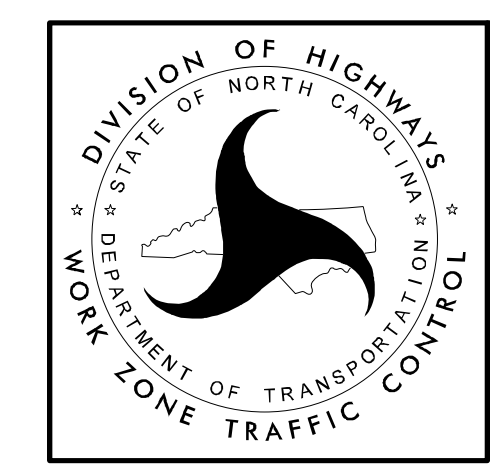
## MAINLINE (-L-) SIGNING

## -Y- LINE SIGNING

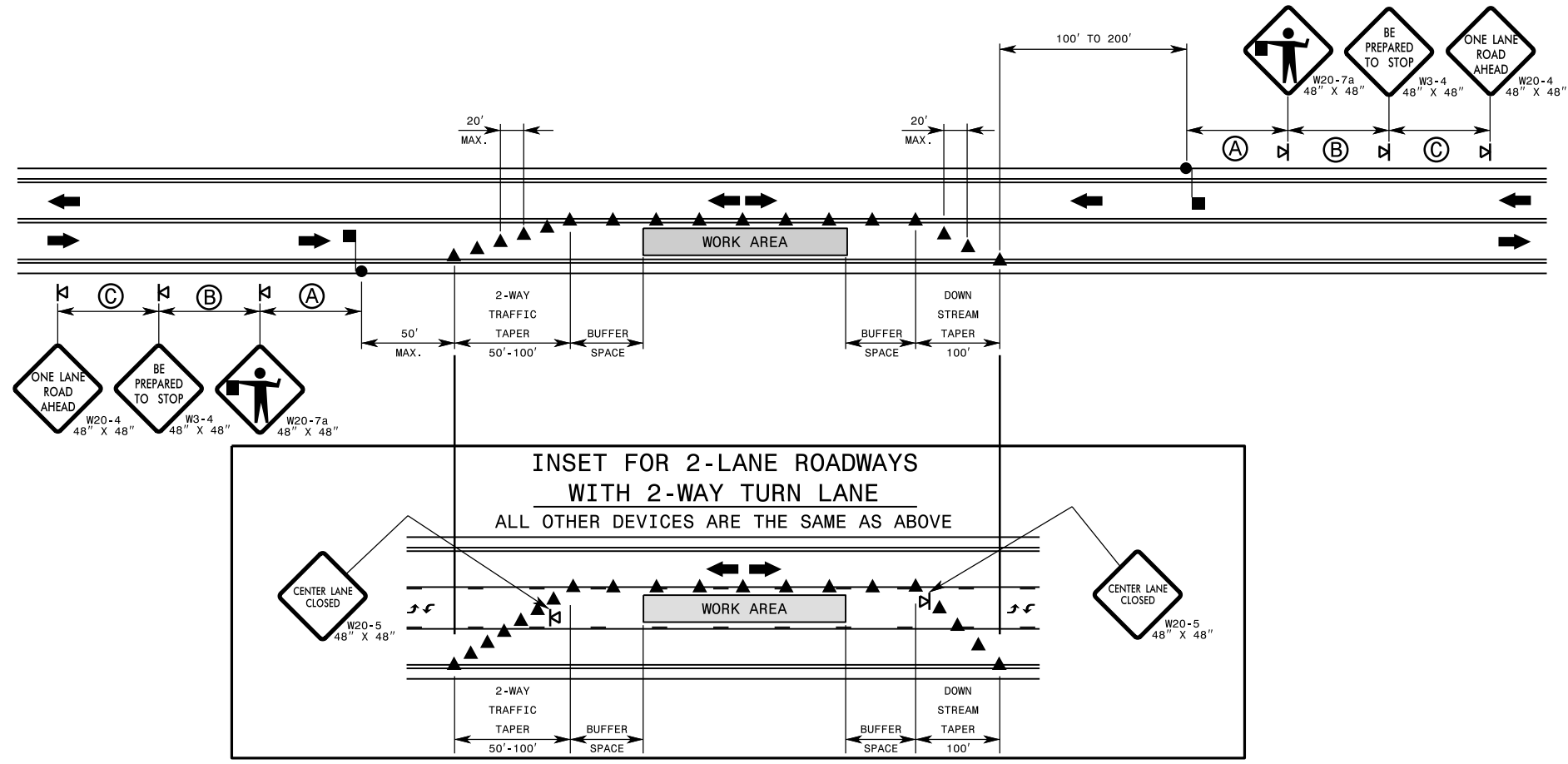
<b>SIGNING NOTES AND PLACEMENT PER DIRECTION</b>		<ul style="list-style-type: none"> <li>- PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</li> <li>- SIGN #2 ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO THE NEAREST WHOLE NUMBER. DO NOT USE FRACTIONAL OR DECIMAL NUMBERS.</li> </ul>	<p><b>STATIONARY SIGNING NOT REQUIRED FOR THE FOLLOWING -Y- LINE CONDITIONS:</b></p> <ol style="list-style-type: none"> <li>1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE</li> <li>2) SUBDIVISION ROADS</li> <li>3) DEAD END ROADS</li> </ol> <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around; align-items: center;"> <div style="text-align: center;"> <p>PLACED 500' IN ADVANCE OF FLAGGER.</p> </div> <div style="text-align: center;"> <p>PLACED 250' IN ADVANCE OF FLAGGER.</p> </div> </div>
		<ul style="list-style-type: none"> <li>- ALTERNATE THE FOLLOWING TWO SIGNS:</li> <li>- STARTING WITH "LOOSE GRAVEL" (W8-7) FOLLOWED BY "UNMARKED PAVEMENT".</li> <li>- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER.</li> <li>- AT TEE INTERSECTIONS INSTALL INITIALLY 0.5 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.</li> </ul>	
		<ul style="list-style-type: none"> <li>- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS.</li> <li>- INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE.</li> <li>- FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH.</li> <li>- A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</li> </ul>	
		<ul style="list-style-type: none"> <li>- PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.</li> </ul>	
	<p>THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.</p>		

### MAPS LESS THAN 2 MILES

FOR AST RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, USE A STATIONARY "LOOSE GRAVEL" SIGN AT THE BEGINNING CONSTRUCTION LIMIT FOLLOWED BY AN "UNMARKED PAVEMENT" SIGN MIDWAY THROUGH AND AN "END ROAD WORK" SIGN AT THE END CONSTRUCTION LIMIT.



**ADVANCE WARNING SIGNS FOR 2-LANE ROADWAY ASPHALT SURFACE TREATMENT**



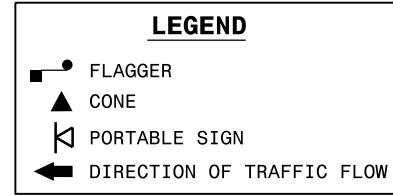
**GENERAL NOTES FOR FLAGGING OPERATIONS**

- 1- REFER TO RSD. 1101.11, SHEETS 1 & 4, FOR "L" DISTANCE AND SIGN SPACING.
- 2- INSTALL LANE CLOSURES WITH THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE UPSTREAM SIDE OF TRAFFIC.
- 3- REMOVE LANE CLOSURES AGAINST THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE DOWNSTREAM SIDE OF TRAFFIC.
- 4- PLACE CONES THRU THE WORK AREA AT THE MAXIMUM SPACING EQUAL IN FEET TO 2 TIMES THE POSTED SPEED LIMIT.
- 5- EXTEND LANE CLOSURES AT THE BUFFER SPACE SUCH THAT STOPPING SIGHT DISTANCE IS PROVIDED TO THE FLAGGER (REFER TO RSD. 1101.11, SHEET 2).
- 6- DO NOT STOP TRAFFIC IN ANY ONE DIRECTION FOR MORE THAN 5 MINUTES AT A TIME.
- 7- DRUMS OR SKINNY DRUMS MAY BE USED IN LIEU OF CONES. REFER TO RSD. 1180.01 FOR SKINNY DRUM REQUIREMENTS.
- 8- USE FLAGGERS TO CONTROL TRAFFIC AT INTERSECTIONS AFFECTED BY THE LANE CLOSURE. SUPPLEMENT FLAGGERS LOCATED AT INTERSECTIONS WITH FLAGGER AHEAD SIGNS (W20-7a) PLACED APPROXIMATELY 250 FT. IN ADVANCE OF THE FLAGGER. FOR SIGNALIZED INTERSECTIONS PLACE SIGNALS IN THE FLASH MODE AND USE LAW ENFORCEMENT.
- 9- REFER TO THE CURRENT MUTCD FOR FLAGGER CONTROL, REQUIREMENTS, AND PROCEDURES.
- 10- DO NOT EXCEED A 1 MILE LANE CLOSURE LENGTH UNLESS OTHERWISE SHOWN IN THE TMP OR AS DIRECTED BY THE ENGINEER.

- 11- IF VEHICLE QUEUES WILL REACH WITHIN 15' OF EITHER SIDE OF ACTIVE RAILROAD TRACKS, PROVIDE A UNIFORMED LAW ENFORCEMENT OFFICER OR FLAGGER TO PREVENT VEHICLES FROM STOPPING WITHIN THE GRADE CROSSING. PROVIDE OFFICER OR FLAGGER EVEN IF AUTOMATIC WARNING MEASURES ALREADY EXIST.

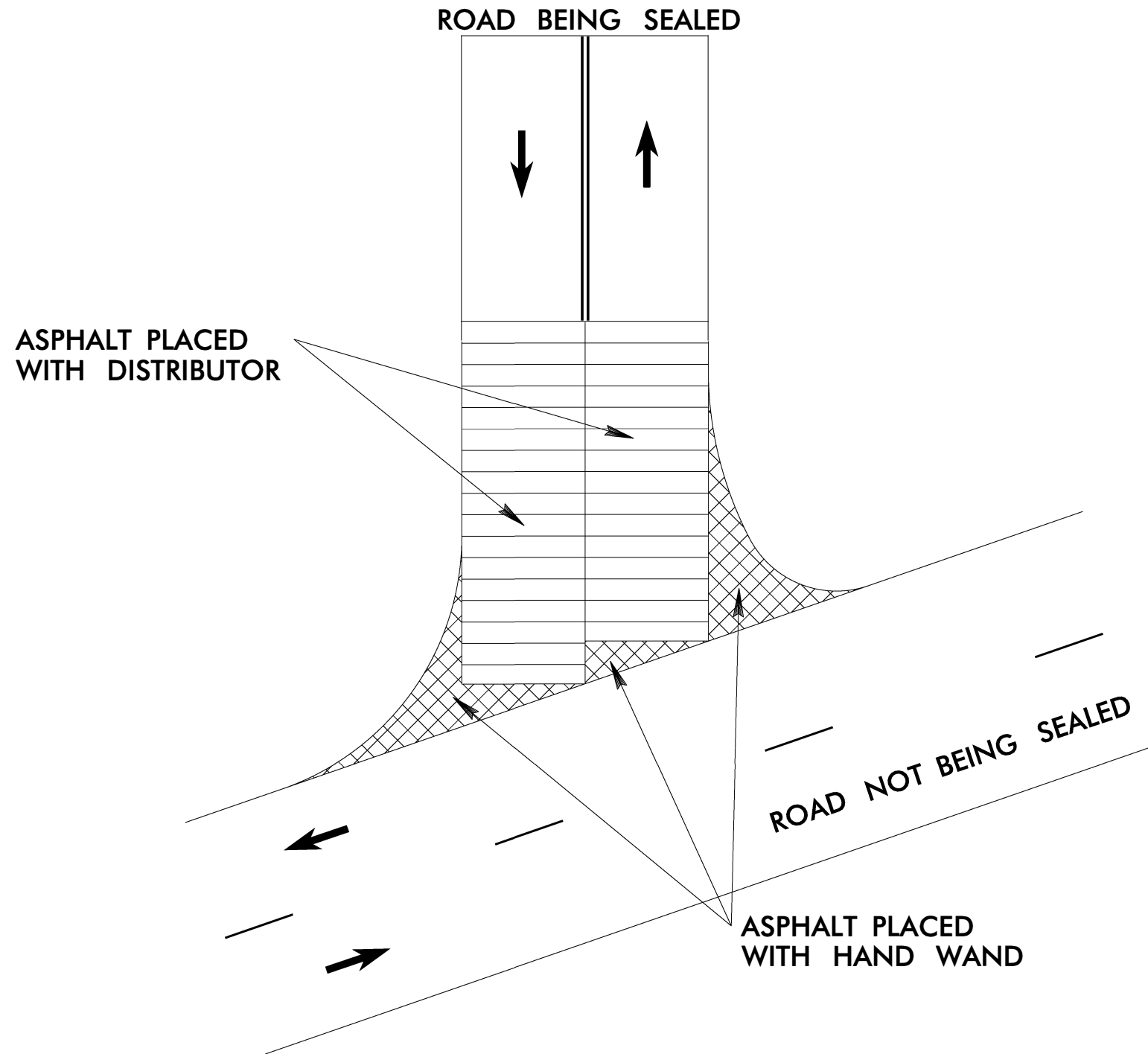
**GENERAL NOTES FOR PILOT CAR OPERATIONS**

- 1- USE PILOT CARS WHEN DIRECTED BY THE ENGINEER.
- 2- IF ROADWAY WIDTH IS LESS THAN 22 FEET (EOP TO EOP), CONES MAY NOT BE REQUIRED ALONG WORK AREA, AND AT THE DISCRETION OF THE ENGINEER, CONES MAY BE OMITTED ALONG THE WORK AREA IF USING A PILOT CAR.
- 3- CONES ARE ALWAYS REQUIRED IN THE UPSTREAM AND DOWNSTREAM TAPERS.
- 4- MOUNT SIGN G20-4 "PILOT CAR FOLLOW ME" AT A CONSPICUOUS POSITION ON THE REAR OF THE PILOT VEHICLE.
- 5- DO NOT INSTALL MORE THAN ONE (1) MILE OF LANE CLOSURE, MEASURED FROM THE BEGINNING OF THE MERGE TAPER TO THE END OF THE LANE CLOSURE.
- 6- ADVISE RESIDENTS AND BUSINESSES WITHIN THE LANE CLOSURE LIMITS ABOUT METHODS OF SAFE EGRESS AND INGRESS FROM DRIVEWAYS DURING FLAGGING AND PILOT CAR OPERATIONS.



# INTERSECTION EMULSION PLACEMENT TWO LANE TWO WAY ROADWAY

PROJECT NO.	SHEET NO.
2022CPT.10.04.20842	8
F.A. PROJECT NO.	



### LEGEND

- ➔ DIRECTION OF TRAFFIC FLOW
- - - SKIP LINES
- === DOUBLE YELLOW LINES

*INTERSECTION EMULSION PLACEMENT*

SCALE	N/A
DATE	9-2019
DWG. BY	TBL
DESIGN BY	TBL
APPROVED	TWB



REVISIONS	